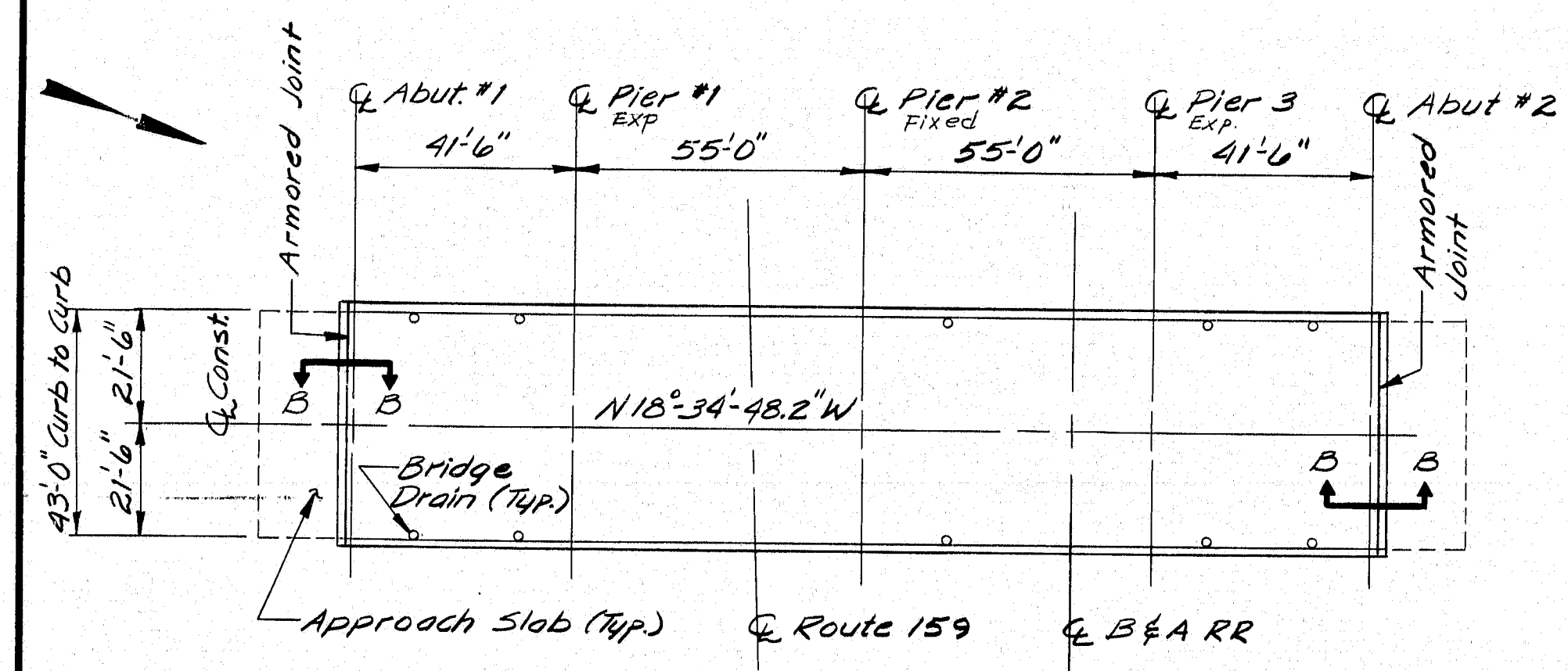
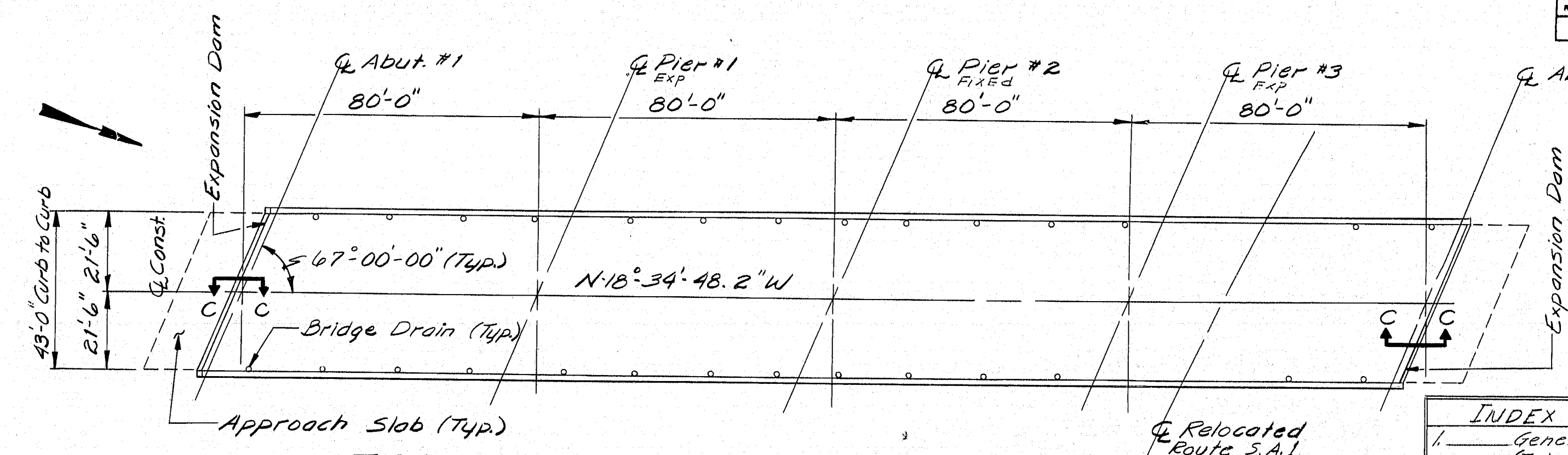


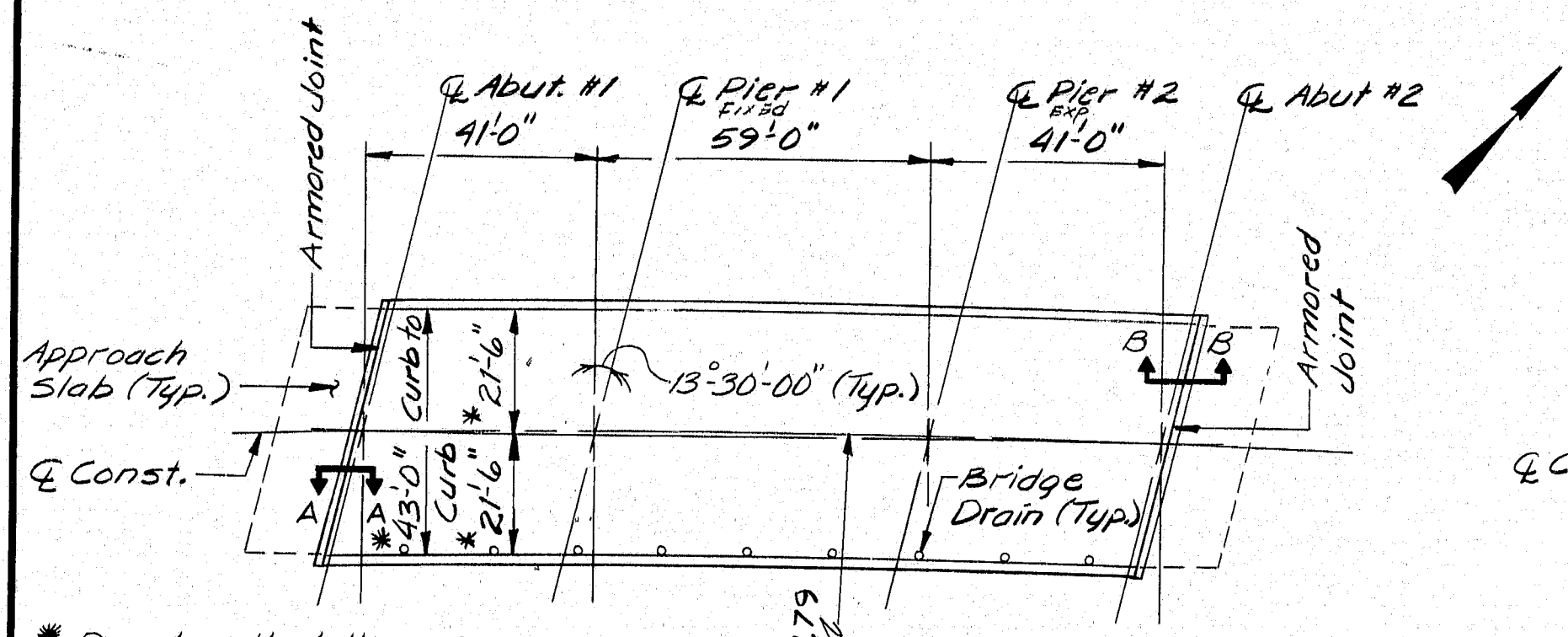
PRJ. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	IR-95-9(109)	26	34



I-95 SOUTHBOUND OVER ROUTE 159 AND B&A RR (6176)

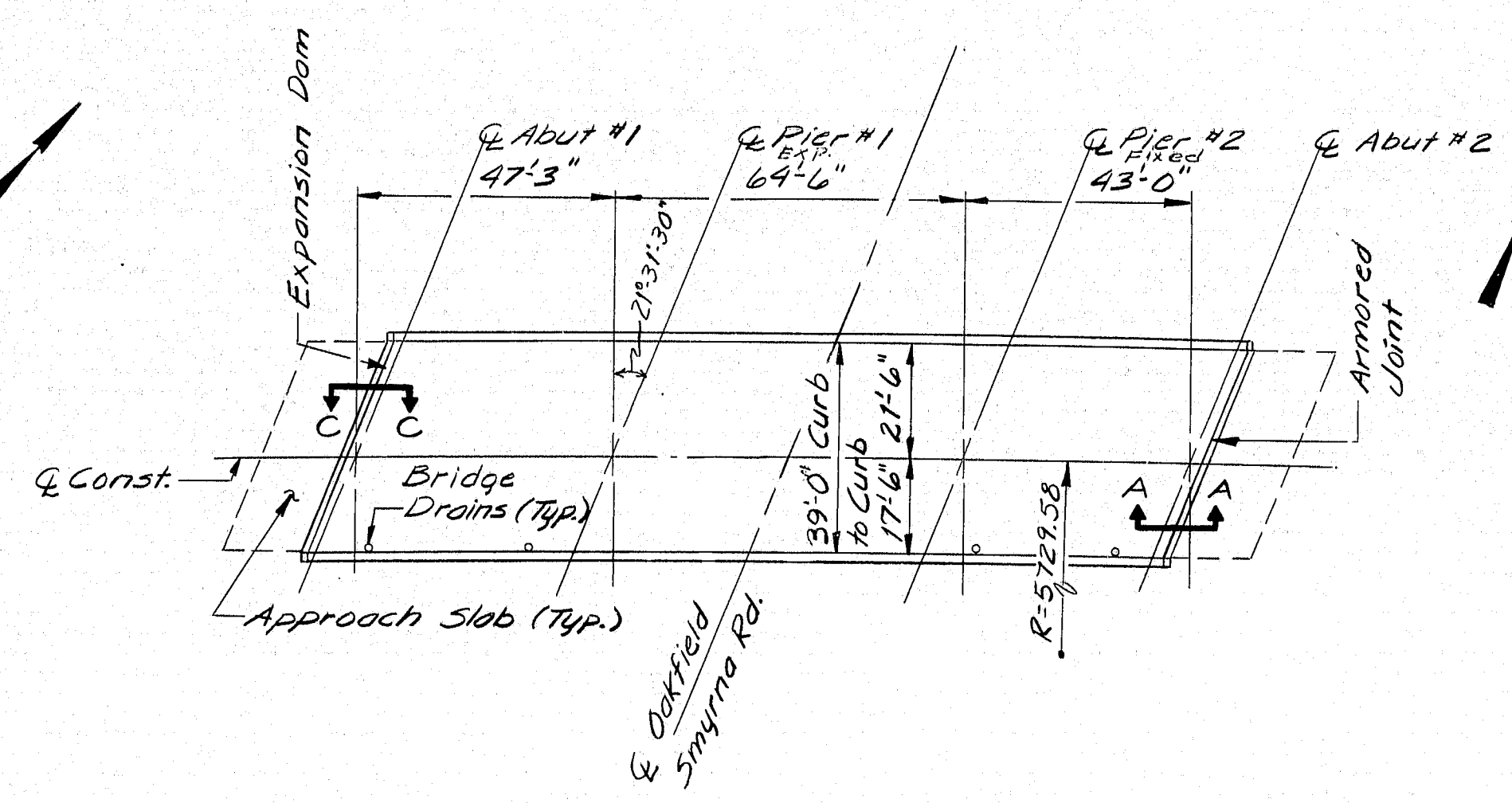


I-95 SOUTHBOUND OVER FISH STREAM AND STATE AID-1 (6177)

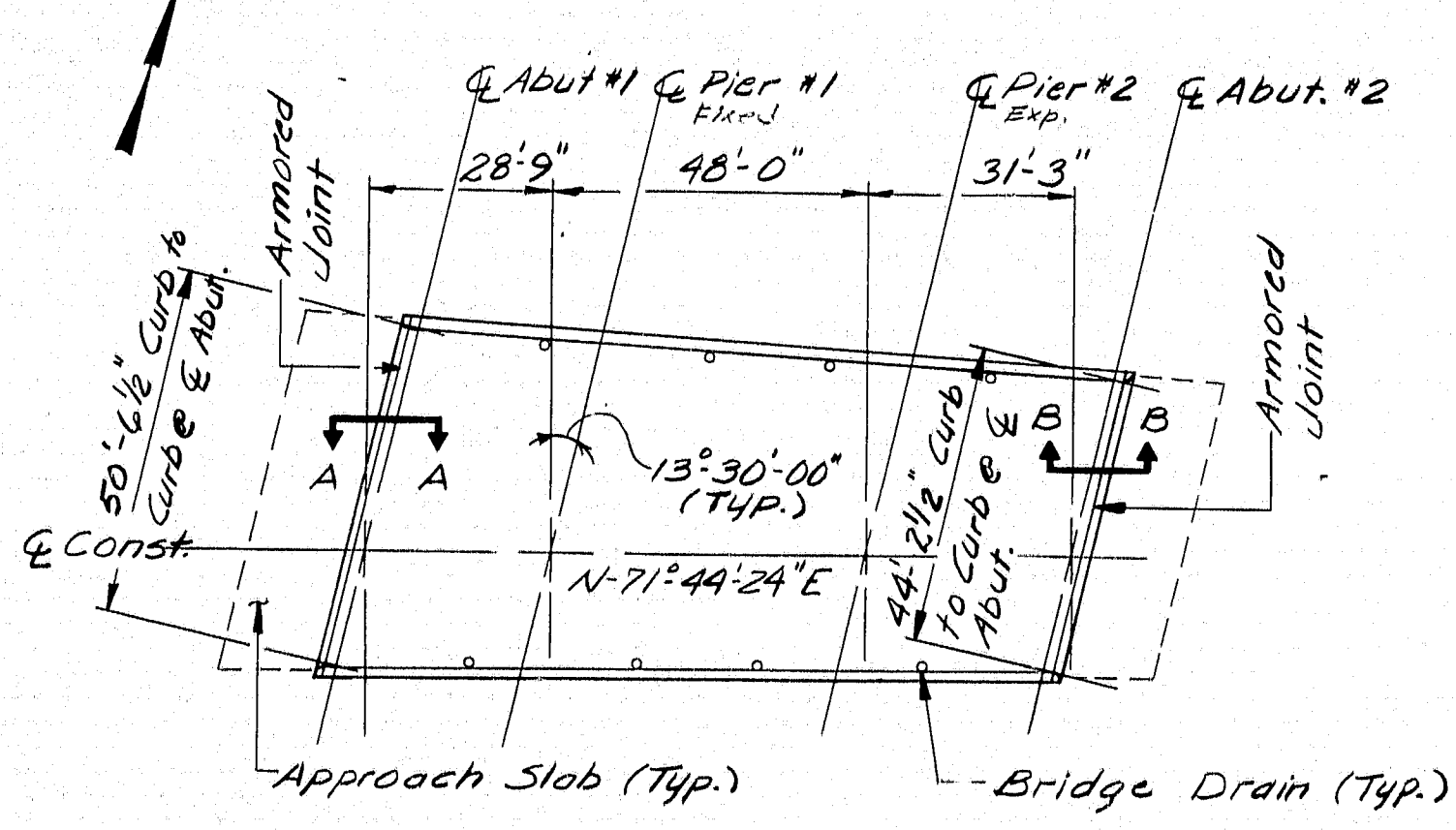


* Denotes that these dimensions were taken from a chord from Abut. #1 to Abut. #2

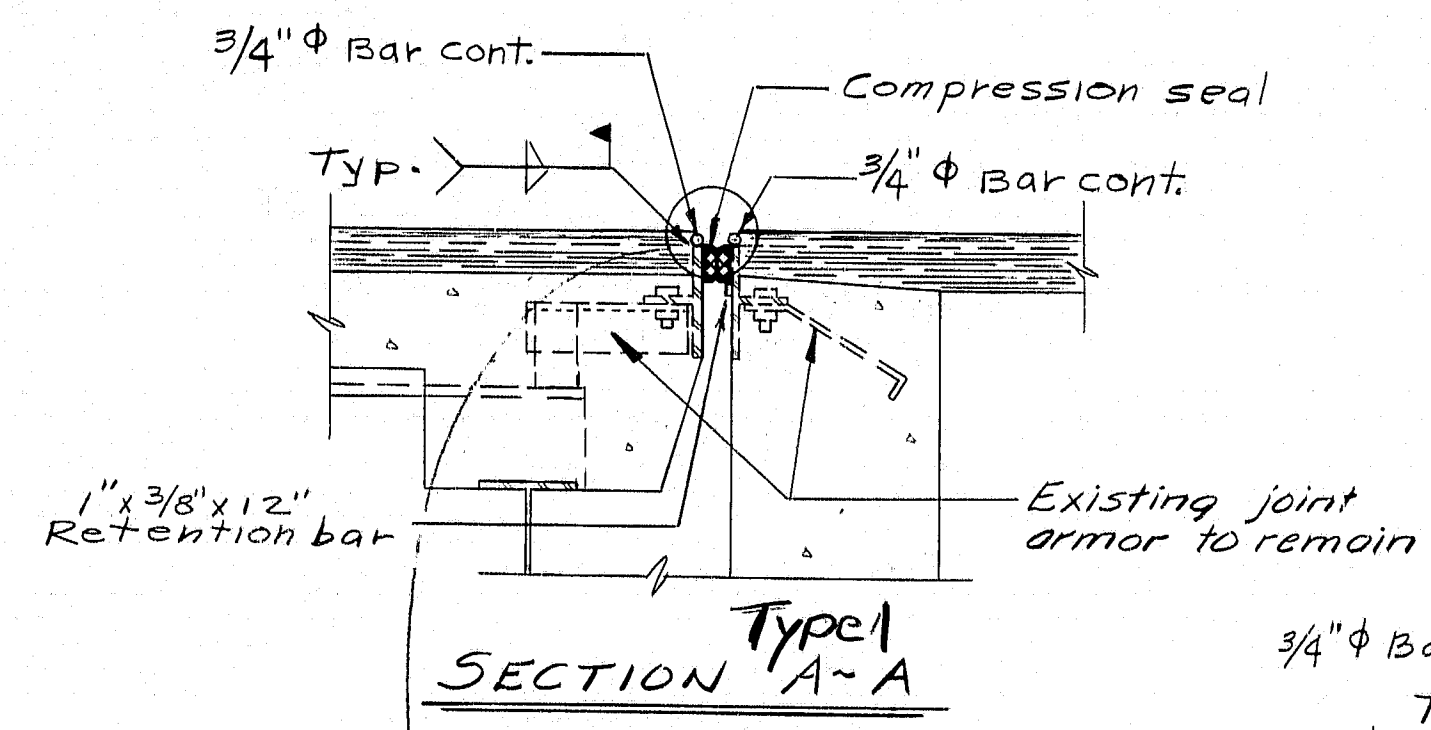
I-95 SOUTHBOUND OVER WEST BRANCH MATTAWAMKEAG RIVER (6178)



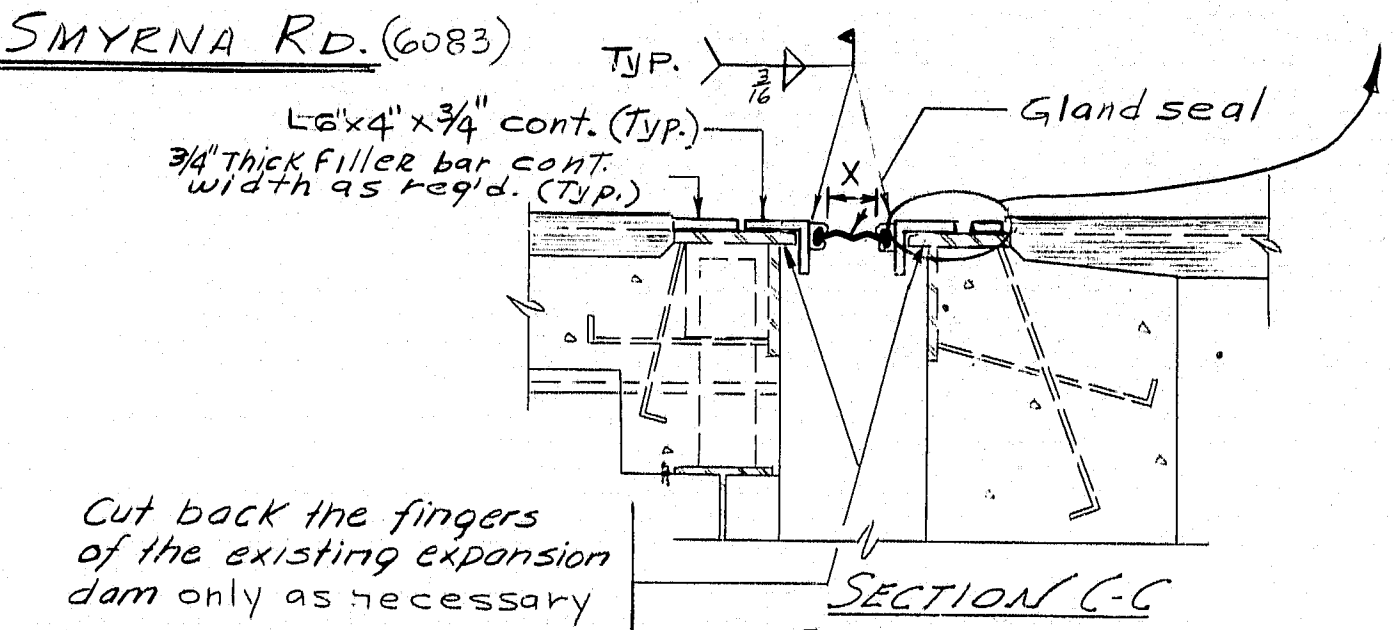
I-95 SOUTHBOUND OVER OAKFIELD - SMYRNA RD. (6083)



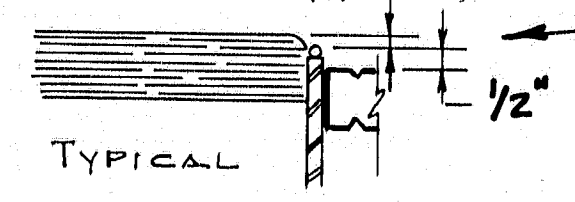
I-95 SOUTHBOUND OVER EAST BRANCH MATTAWAMKEAG RIVER (6084)



SECTION A-A

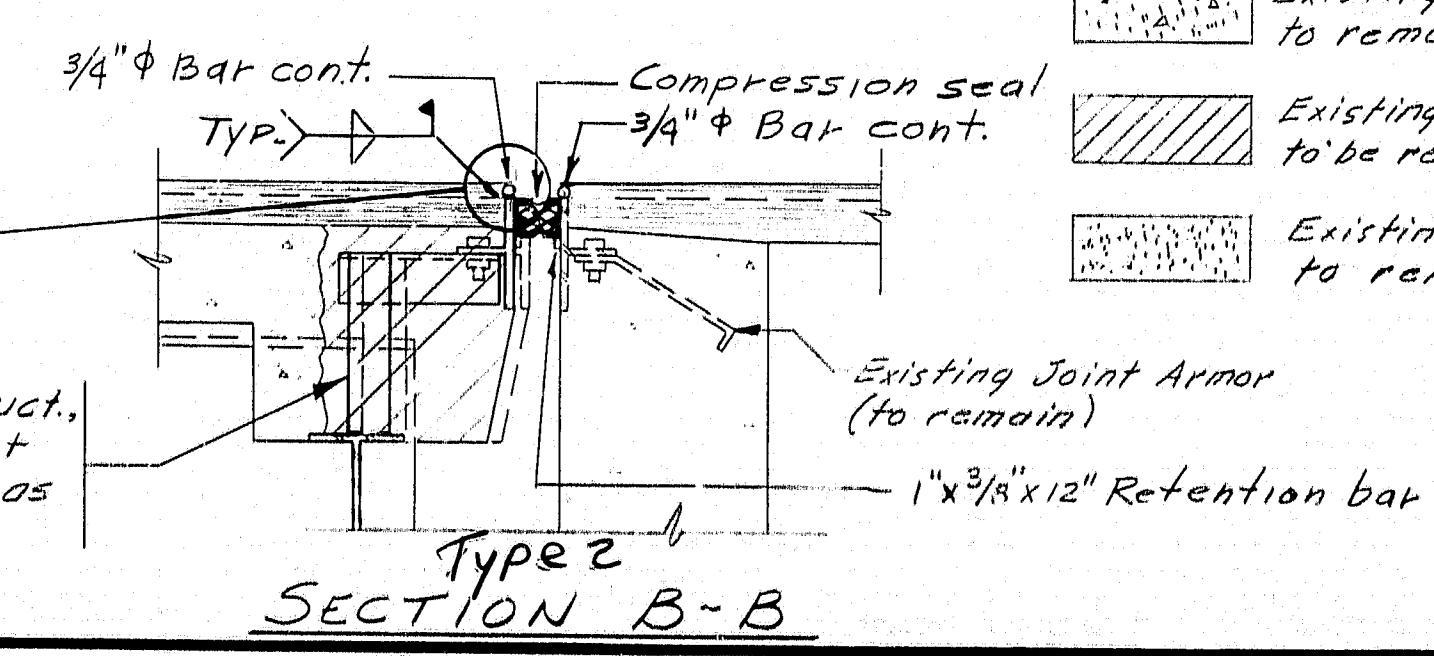


SECTION C-C



TYPICAL

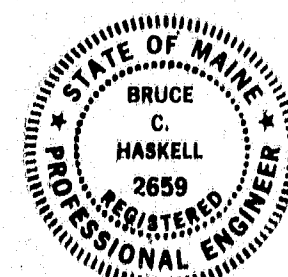
Cut back, superstructure, and remove and reset existing joint armor as necessary.



SECTION B-B

LEGEND

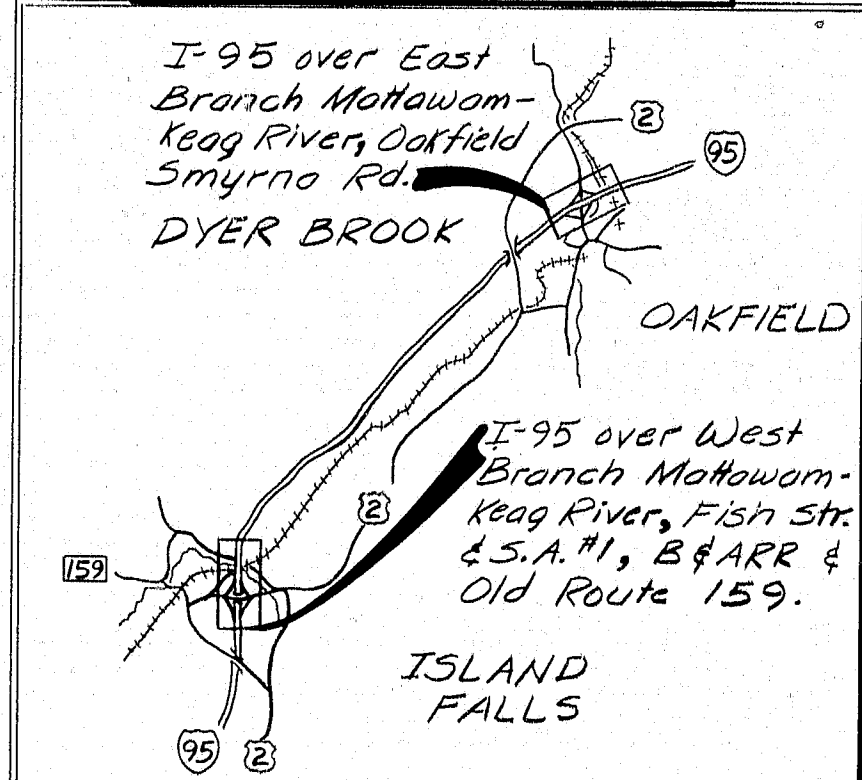
- Existing concrete to remain
- Existing concrete or granite to be removed and rebuilt
- Existing granite to remain



INDEX OF BRIDGE SHEETS	
1.	General Plan & Sections (Island Falls, Oakfield)
2.	General Plan & Sections (Oakfield - Smyrna)
3.	Half Sections & Notes
4.	Joint Modification - End Post & Rail Anchor Detail (Route 159 & B&A RR SB)
5.	Joint Modification - End Post & Rail Anchor Detail (Fish Stream & State Aid-1 SB)
6.	Joint Modification - End Post & Rail Anchor Detail (Fish Stream & State Aid-1 SB)
7.	Reinforcing Steel Schedule

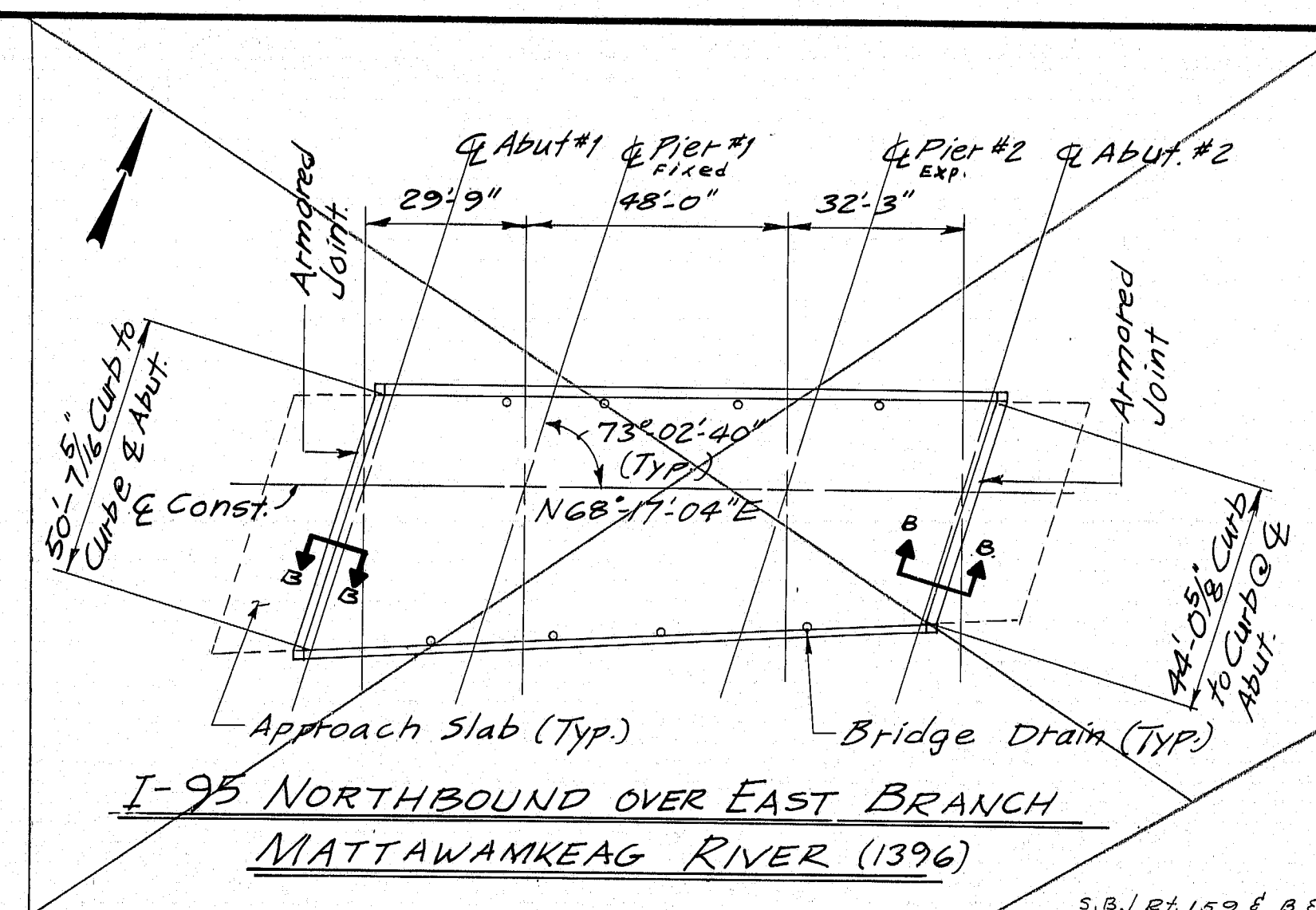
BRIDGE STANDARDS	
1.	BD 125-82 Compression and Glue Seal
2.	BD 127-81 Temporary Concrete
3.	BD 201-89 Concrete End Post
4.	BD 301-89 Compression Seal
5.	BD 302-89 Glue Seal

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE



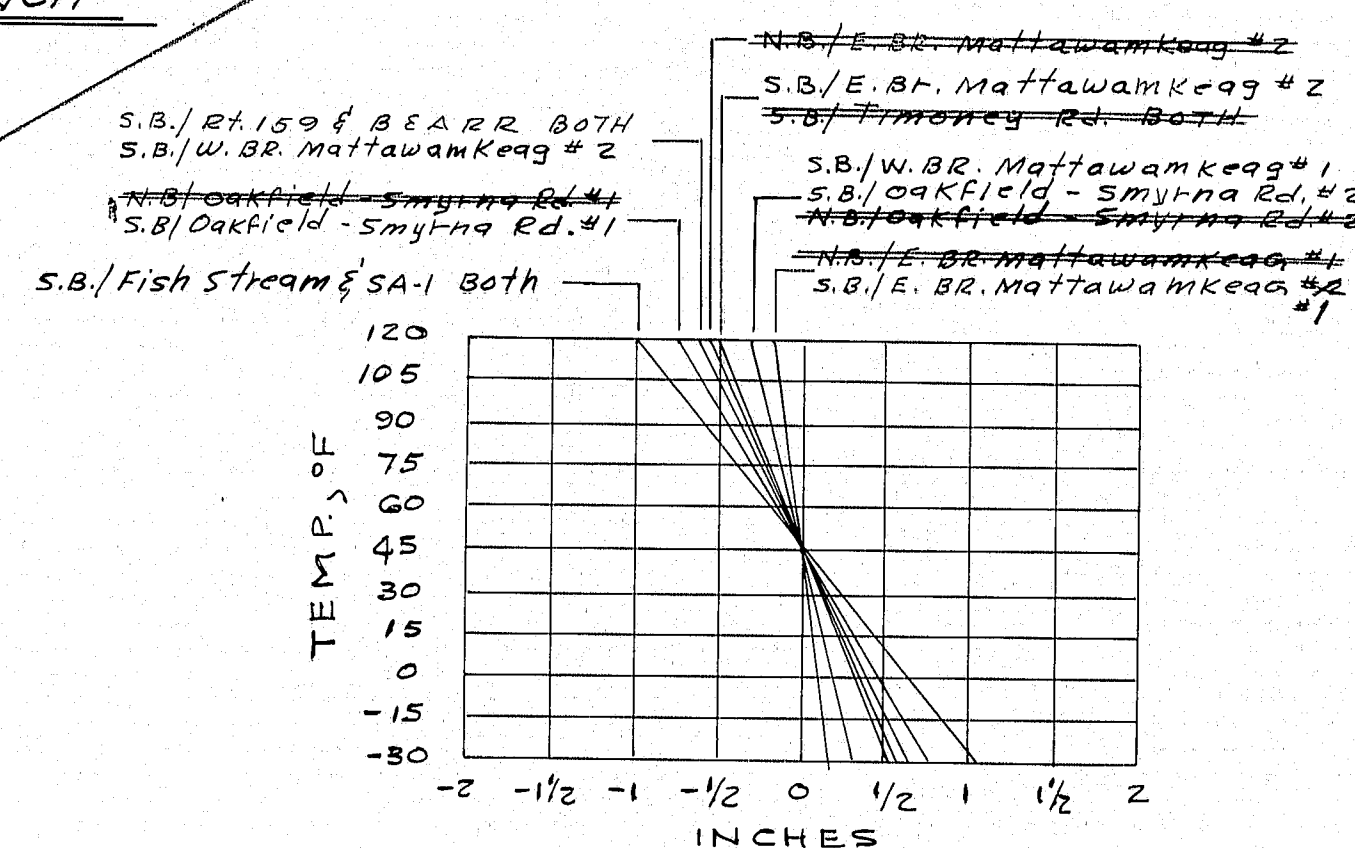
LOCATION MAP
0 1 2 3 4 5
Scale in miles

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
INTERSTATE 95 SOUTHBOUND OVER
East Branch Mattawamkeag River, Oakfield - Smyrna Rd., West Branch Mattawamkeag River, Fish Stream & State Aid-1, Bangor & Arcoostook RR & Old Route 159
ISLAND FALLS - OAKFIELD
General Plan & Sections
SHEET 1 OF 89 AUGUSTA, MAINE DEC. 1984

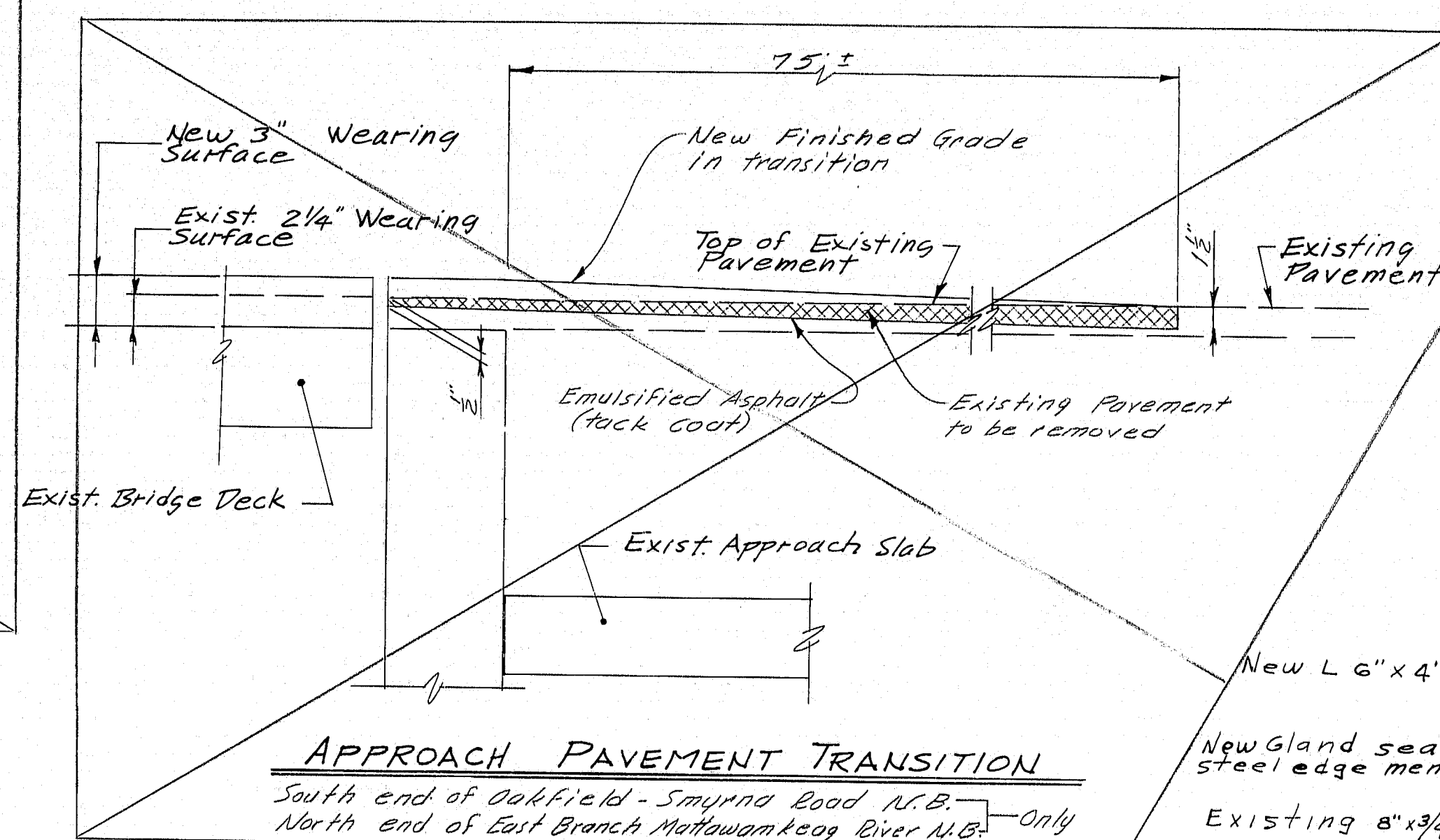


F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO	TOT. SHEETS
IR-IR-95 7(10)	MAINE	IR-95-7(109)	27	32

1. SEALS TO BE FURNISHED SHALL HAVE A MOVEMENT RATING OF:
- 2" - SB/FISH STREAM & STATE AID-1 (BOTH)
 - 1 1/2" - SB/OAKFIELD/SMYRNA ROAD (#1)
 - 1 1/2" - RB/OAKFIELD/SMYRNA ROAD (#2)
 - 1 1/4" - SB/ROUTE 159 AND B&ARR (BOTH)
 - 1 1/4" - SB/W. BR. MATTAWAMKEAG (#2)
 - 1 1/4" - NB/E. BR. MATTAWAMKEAG (#1)
 - 1" - SB/E. BR. MATTAWAMKEAG (#2)
 - 1" - SB/LIMMER ROAD (BOTH)
 - 5/8" - SB/W. BR. MATTAWAMKEAG (#1)
 - 5/8" - SB/OAKFIELD/SMYRNA ROAD (#2)
 - 5/8" - NB/OAKFIELD/SMYRNA ROAD (#1)
 - 3/4" - SB/E. BR. MATTAWAMKEAG (#1)
 - 3/4" - NB/E. BR. MATTAWAMKEAG (#1)
2. SEALS SHALL BE APPROVED BY THE ENGINEER PRIOR TO FABRICATION OF THE JOINT ARMOR.
3. COMPRESSION SEAL JOINT OPENINGS WILL VARY DEPENDING ON THE DIMENSIONS OF THE SEALS SELECTED BY THE CONTRACTOR. THE JOINT OPENING SHALL BE SET ACCORDING TO THE OPENING SHOWN ON THE APPROVED SHOP DETAIL DRAWINGS.
4. THE COMPRESSION SEAL ADJUSTMENT CHART SHOWS THE ADJUSTMENT NECESSARY TO ADJUST THE JOINT OPENING SHOWN ON THE SHOP DETAIL DRAWINGS (FOR COMPRESSION SEALS) OR IN THESE DRAWINGS (FOR GLAND SEALS) FOR TEMPERATURES OTHER THAN 70°F. ADJUSTMENT IS TO BE MEASURED PARALLEL TO THE CENTERLINE OF CONSTRUCTION.

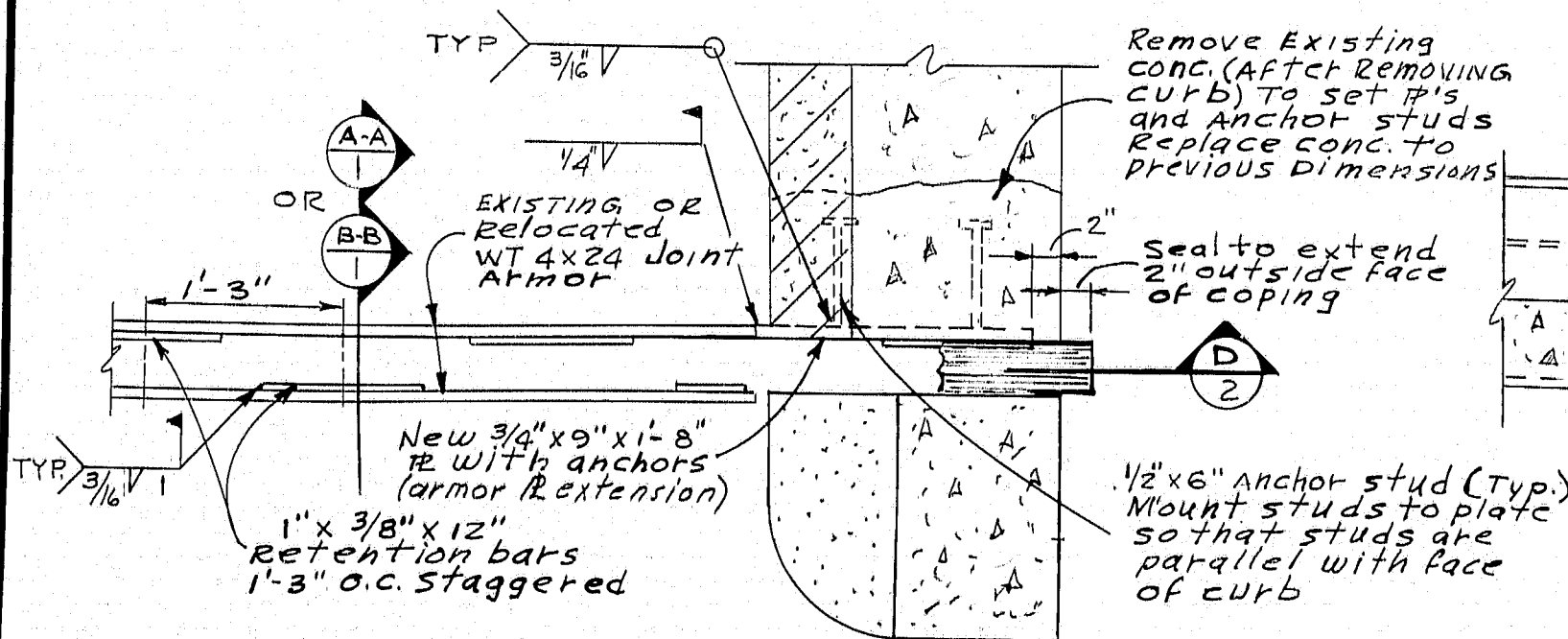


SETTING



OPTIONAL TREATMENT FOR GRANITE CURB END PIECE

THE EXISTING GRANITE CURB END PIECE MAY BE REMOVED AND RESET, IF POSSIBLE, AT THE ADJUSTED LOCATION. EXISTING UNITS ARE SECURED WITH TWO A606 DOWELS LOCATED 1/4" FROM THE FACE OF THE GRANITE, THEN SET INTO DRILLED HOLES IN THE CONCRETE BELOW. IF RE-USED, RESET THE DOWELS IN A SIMILAR MANNER. IF NOT RE-USED, PROVIDE NEW GRANITE IN A PIECES AS SHOWN IN THE DETAILS AT LEFT, AND ANCHOR IN A SIMILAR MANNER.



PARTIAL PLAN - COMPRESSION SEAL

~~See BD 124-81~~
See BD 301-89

APPROACH PAVEMENT TRANSITION

South end of Oakfield - Smyrna Road N.B. — Only
North end of East Branch Mattawamkeag River N.B.

SCOPE OF WORK

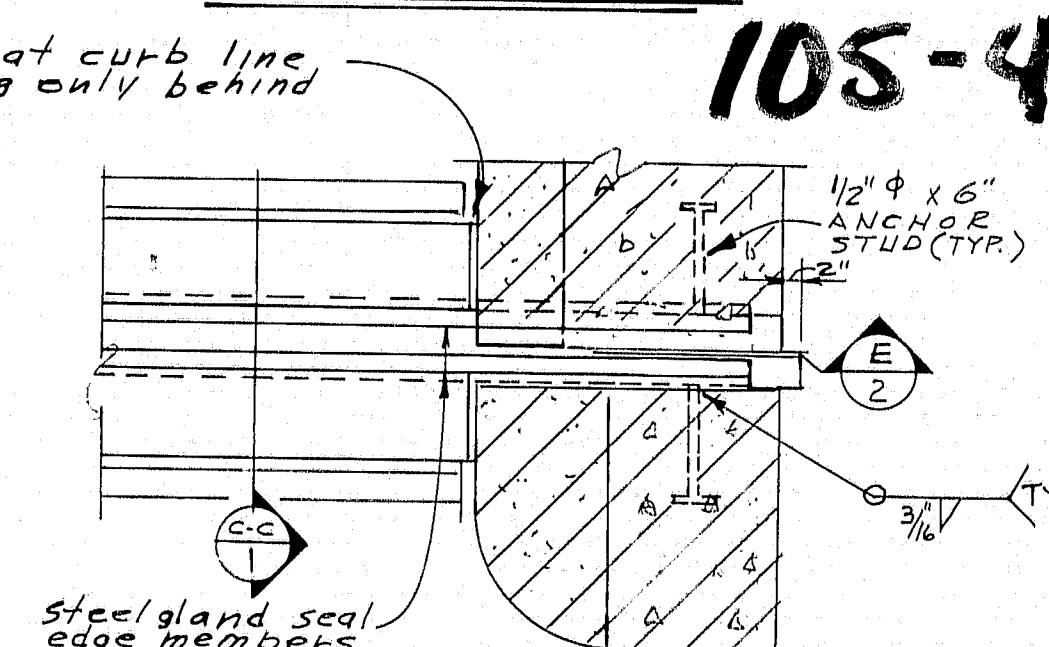
- 2- Remove existing 2" bituminous concrete wearing surface and membrane waterproofing. Replace with new membrane waterproofing and 3" of hot bituminous wearing surface.

- Modify all existing open joints at abutments by installing compression seal or gland seal.

- Remove and rebuild existing end posts, as shown, with guard rail connection.

- Clean & Paint all Structural Steel

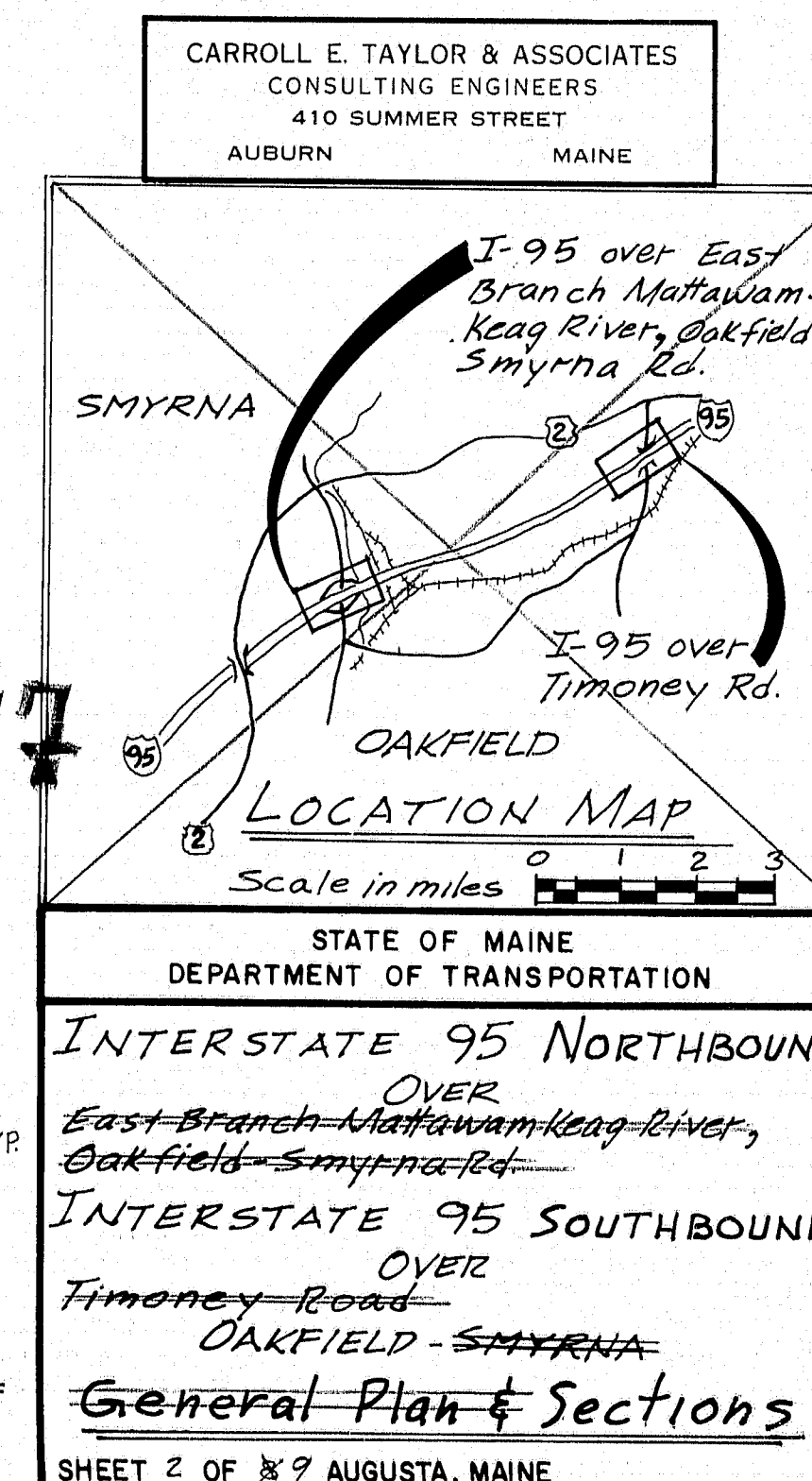
SECTION E

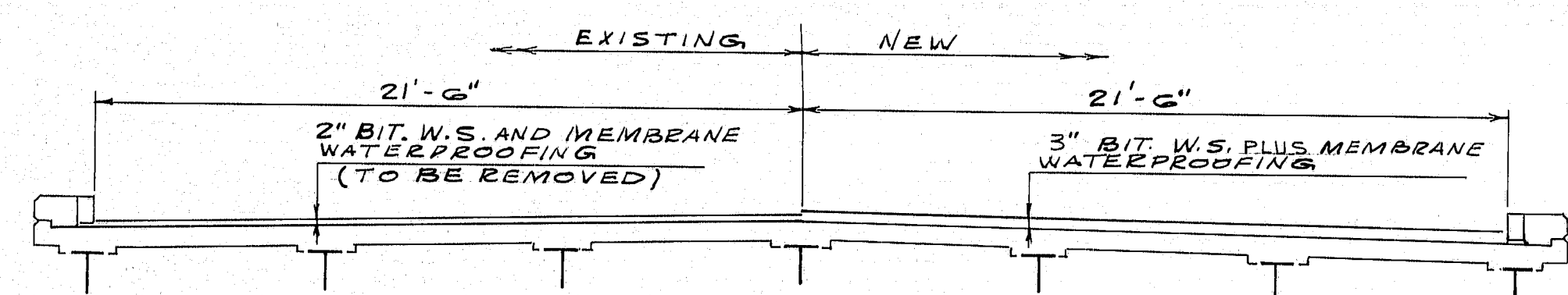


PARTIAL PLAN - GLAND SEAL

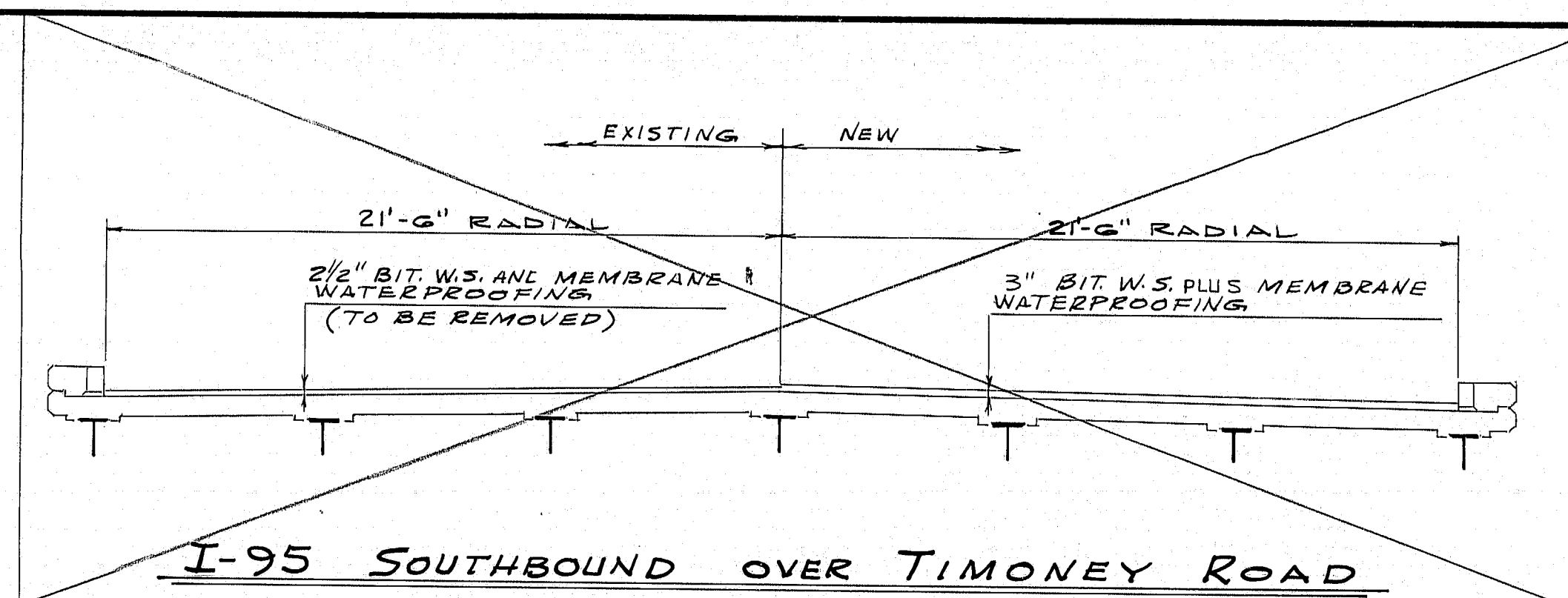
~~SEE BD 124-81~~
See BD 302-89

1-95-9 (109) OAKFIELD

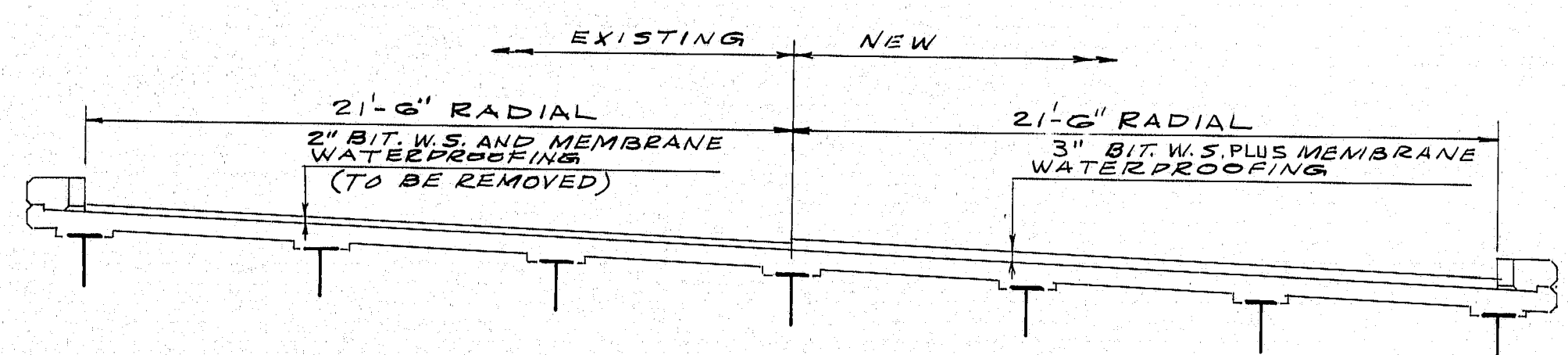




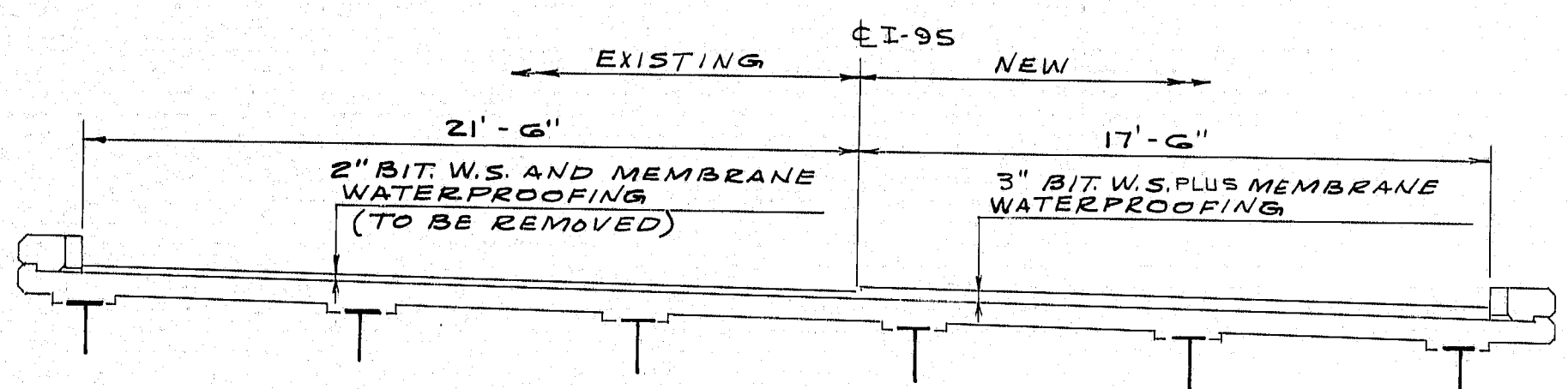
I-95 Southbound over Route 159 and B & ARR
I-95 Southbound over Fish Stream and State Aid-1



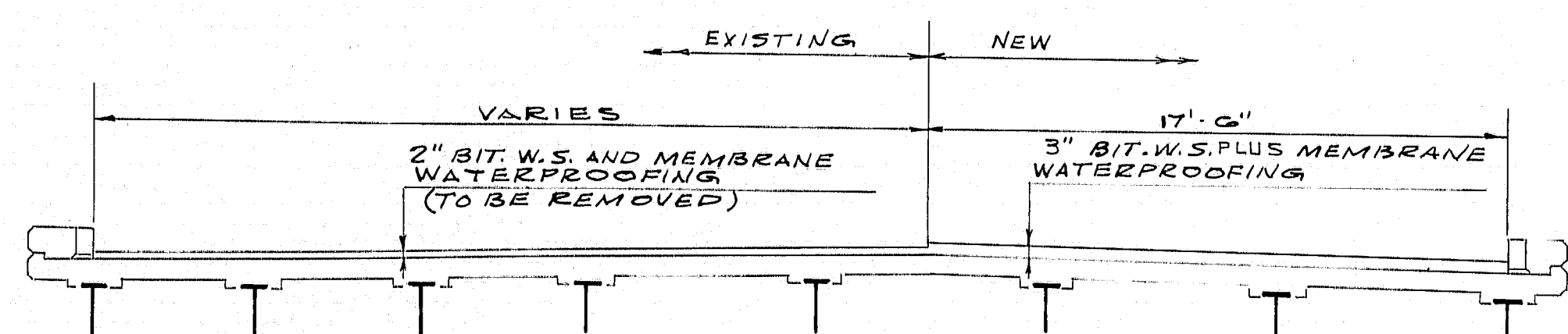
I-95 Southbound over Timoney Road



I-95 Southbound over West Branch Mattawamkeag River



I-95 Southbound over Oakfield - Smyrna Road (Shown)
I-95 Northbound over Oakfield - Smyrna Road (Crown at E)



I-95 Southbound over East Branch Mattawamkeag River
I-95 Northbound over East Branch Mattawamkeag River

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
MAINE	IR-95-9(109)	28	34

CONSTRUCTION NOTES

1. MAINTAIN ONE 12-FOOT MINIMUM TRAFFIC LANE AT ALL TIMES.
2. ALL WORK SHALL BE DONE BEHIND TEMPORARY CONCRETE BARRIERS.
3. THE TOP SURFACE OF THE EXISTING CONCRETE SLABS SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER.
4. DEPRESS THE BITUMINOUS WEARING SURFACE AROUND THE EXISTING BRIDGE DRAINS AS DIRECTED BY THE ENGINEER.
5. BECAUSE OF STAGED CONSTRUCTION, SOME EXPANSION JOINTS MAY REQUIRE CONSTRUCTION JOINTS. THESE SHALL BE AS APPROVED BY THE ENGINEER AS TO TYPE AND LOCATIONS.
6. ~~PAYMENT FOR DRILLING AND GROUTING ASSOCIATED WITH MODIFICATIONS OF THE JOINTS WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.~~
7. ~~PAYMENT FOR REMOVING AND RE-INSTALLING BRIDGE RAIL OR GUARDRAIL AS NEEDED TO ACCOMPLISH JOINT MODIFICATIONS, WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.~~
8. ~~PAYMENT FOR REMOVING AND REPLACING PORTIONS OF END POSTS WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.~~
9. PAYMENT FOR CUTTING, REMOVING, OR REPLACING GRANITE CURB WHERE CALLED FOR WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
10. REINFORCING STEEL SHALL HAVE A MINIMUM COVER OF 2 INCHES UNLESS NOTED OTHERWISE.
11. WHERE GRANITE CURB IS CALLED FOR TO BE REMOVED TO MODIFY A JOINT OR TO INSTALL A SEAL, ONLY THE SMALLEST AMOUNT NECESSARY TO ACCOMPLISH THE WORK SHALL BE REMOVED. IT IS PREFERABLE TO REMOVE CURB TO AN EXISTING JOINT, BUT IF A JOINT IS MORE THAN 3 FEET AWAY THE CURB SHALL BE SAW CUT IN THE FIELD. THE DECISION ABOUT WHETHER TO CUT OR NOT, AND WHERE, SHALL BE THE ENGINEER'S.
12. ~~THE REMOVAL OF THE EXISTING BITUMINOUS PAVEMENT ON THE APPROACHES SHALL BE INCIDENTAL TO ITEM 202-127.~~
13. The following labor and material shall be paid for under Item 606.173 Bridge Connection.
 - Removal of existing concrete end posts.
 - Removing and re-installing existing bridge rail posts.
 - Cutting existing bridge rail.
 - Concrete for new end posts.
 - All hardware for approach rail anchor.
 - W-Beam Terminal Connector (EE-S-19).
 - Remove, reset and add new guard rail posts.
 - Additional beam rail panels, with extra holes.
 - Additional holes in existing beam rail panels.
 - Protective Coating for Concrete.
14. Additional holes required in guard rail panels 2" x 8" may be made by drilling, punching, or any other method that produces a neat clean hole of the required size. Burning of holes will not be allowed.

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 CONSULTING ENGINEERS
 410 SUMMER STREET
 AUBURN MAINE

STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION

HALF SECTIONS & NOTES

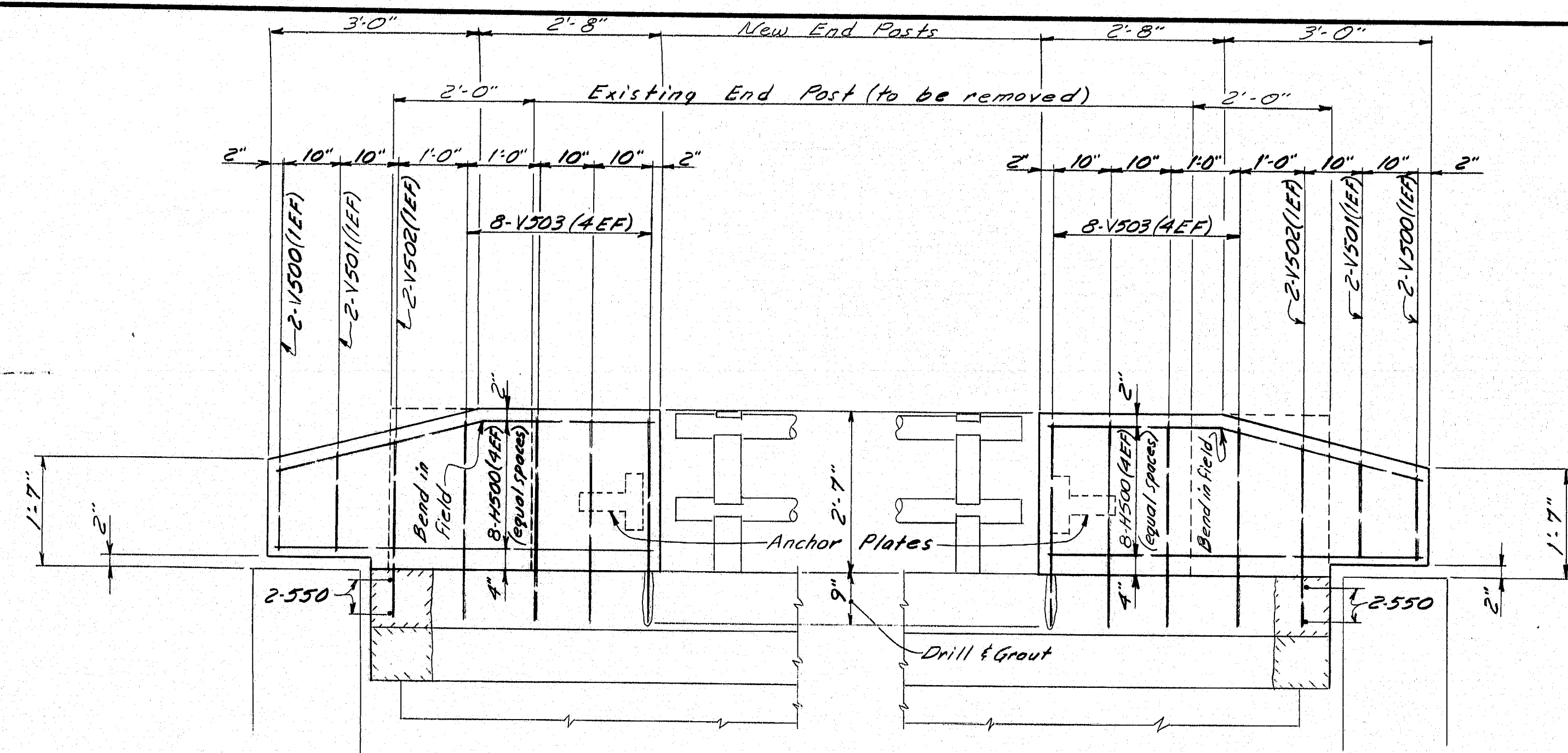
105-48

For notes see sheet #5

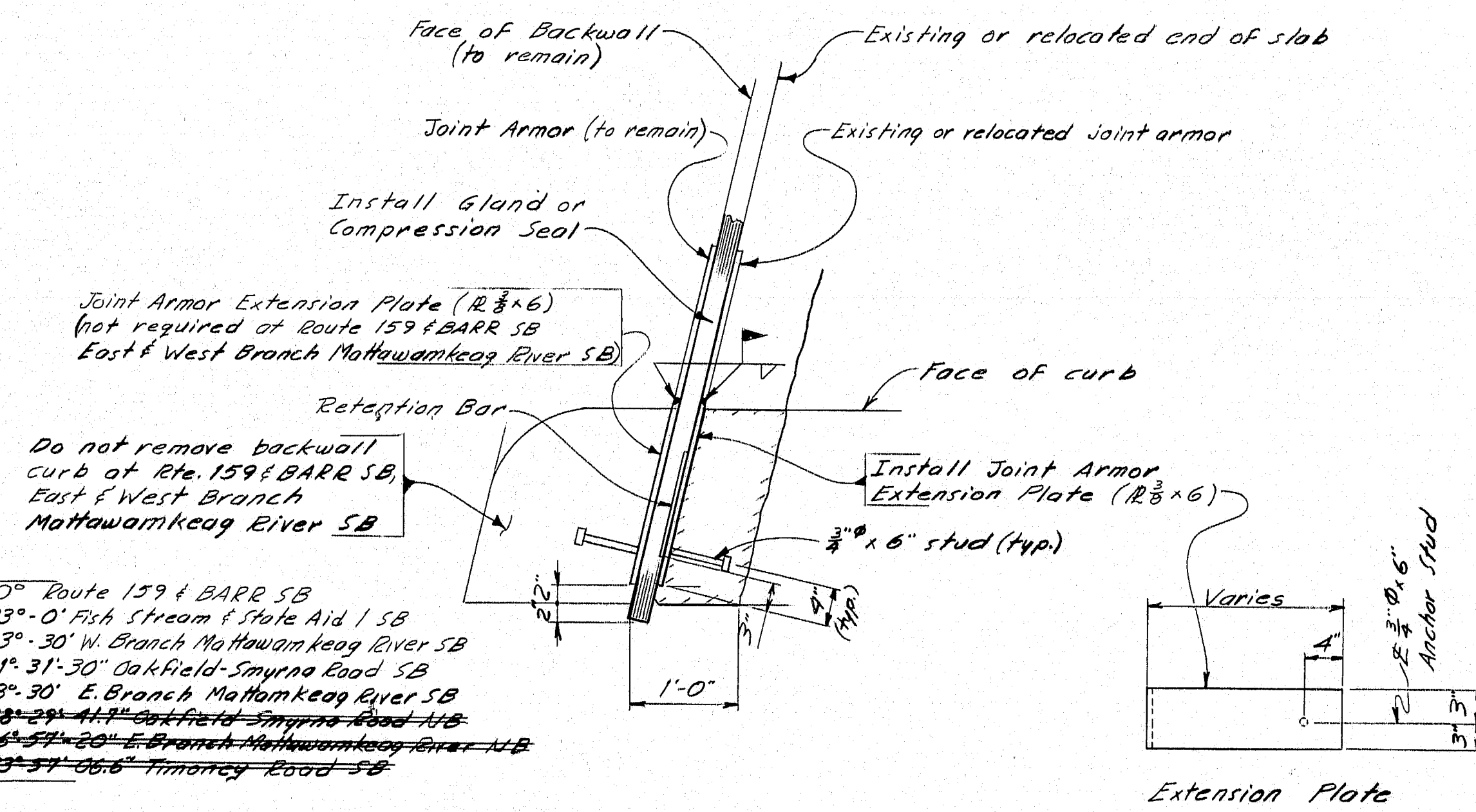
PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAIL		
REVISIONS		
FIELD CHANGES		

BRIDGE 14-112 4/7/01

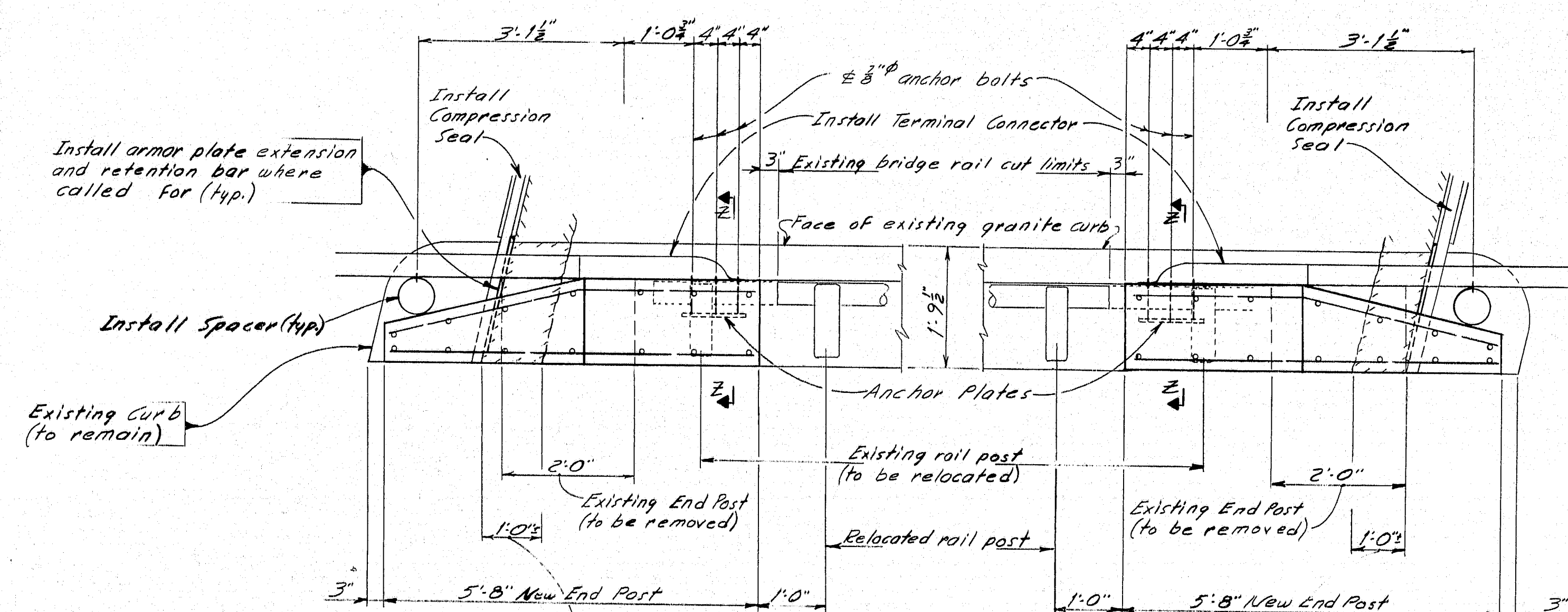
F.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	IP-95-9(109)	29	34



ELEVATION
View shown: Route 159 and BARR SB - East Side, West side opposite hand
W. Branch Mattawamkeag River SB - South side, North side opposite hand
E. Branch Mattawamkeag River SB



JOINT ARMOR EXTENSION PLATE DETAIL



At Abut #1 W. Branch Mattawamkeag River SB and Abut #1 E. Branch Mattawamkeag River SB existing concrete and granite curb need not be removed to install compression seal if so directed.

Remove portions of existing concrete and granite curb, concrete slab to limits shown. Cut back existing reinforcing steel only to the extent required to make modifications. Place additional reinforcing steel as shown. Install armor plate extension and compression seal. Rebuild concrete curb and slab to accommodate new seal. Abut #1 & #2 Rte. 159 & BARR SB
Abut #2 W. Branch Mattawamkeag River SB
Abut #2 E. Branch Mattawamkeag River SB

PLAN
East Branch Mattawamkeag River shown, others similar

Work this sheet with:
BD 201-89 Concrete End Post
BD 301-89 Compression Seal
BD 302-89 Gland Seal

For notes see sheets #3 and #5

For Notes
For Section Z-Z
For Anchor Plate Detail
For Anchor Bolt Detail
For Spacer Detail
For Guard Rail Layout

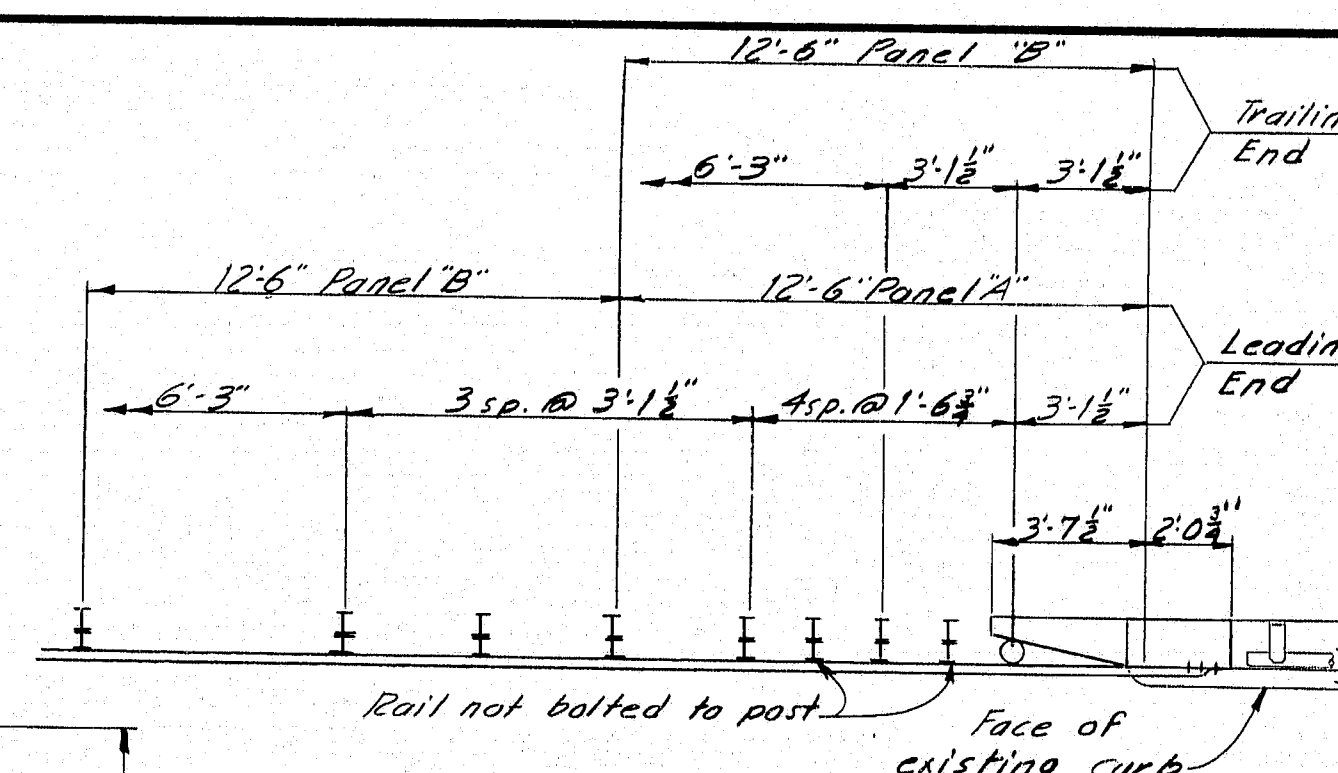
STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

JOINT MODIFICATION
END POST & RAIL ANCHOR DETAIL

INTERSTATE 95 OVER:
Route 159 and B&A RR SB
W. Branch Mattawamkeag River SB
E. Branch Mattawamkeag River SB

SHEET 4 OF 9 AUGUSTA, MAINE

105-49

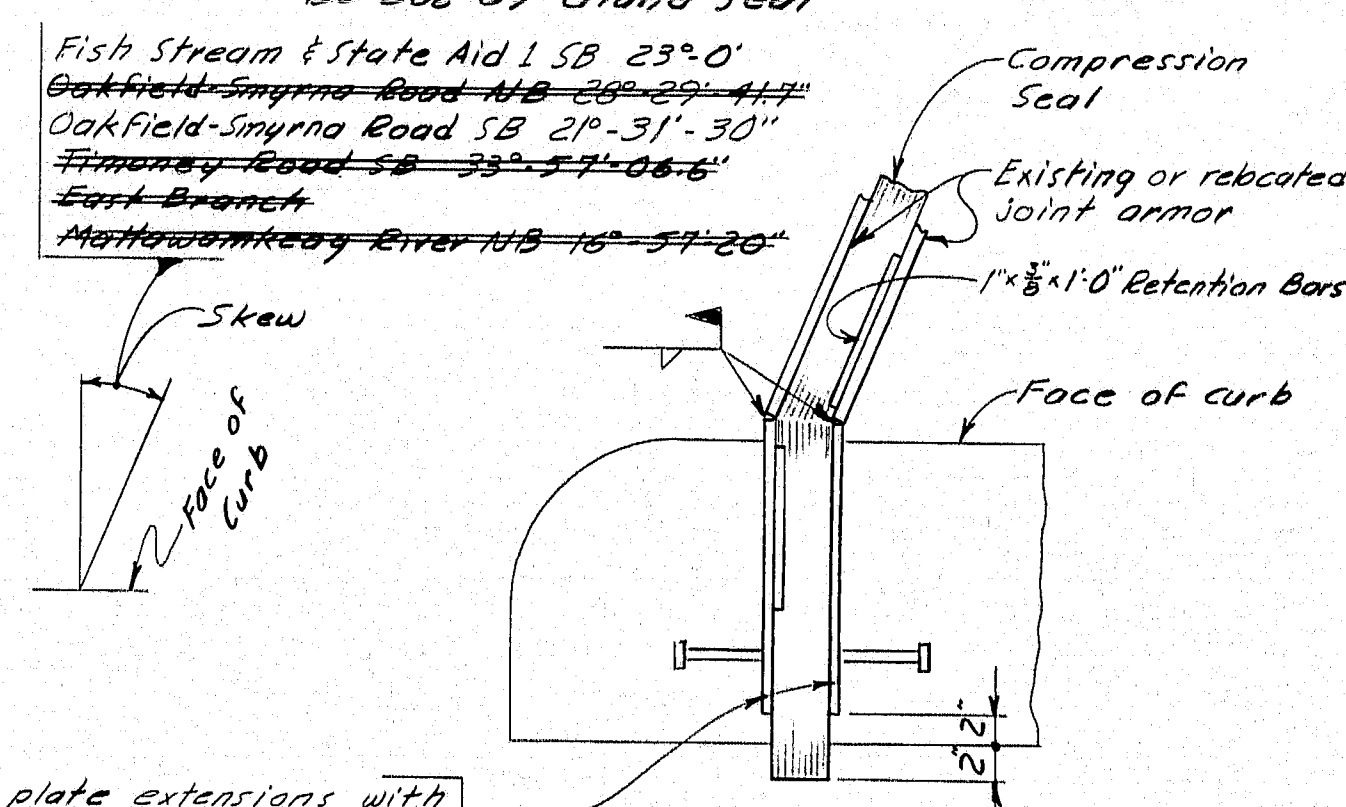
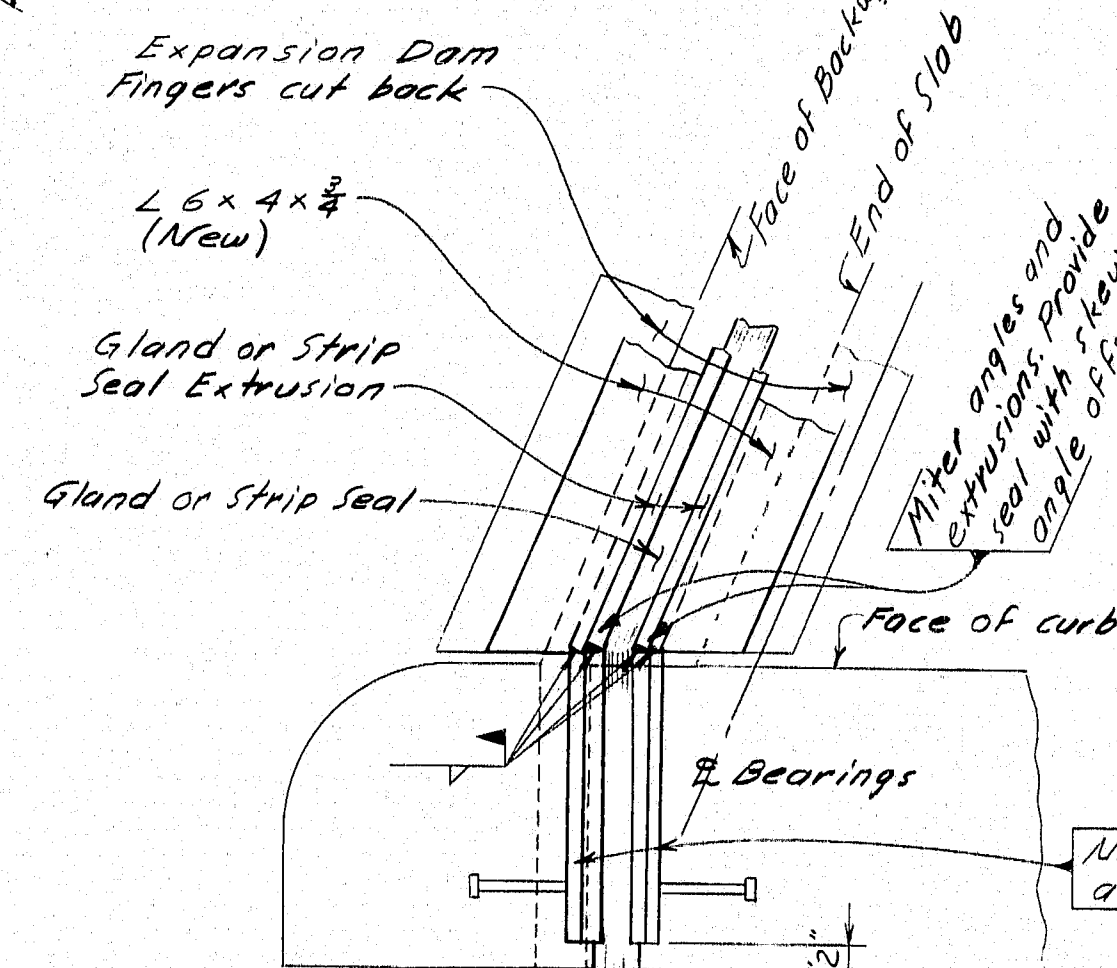


NOTES

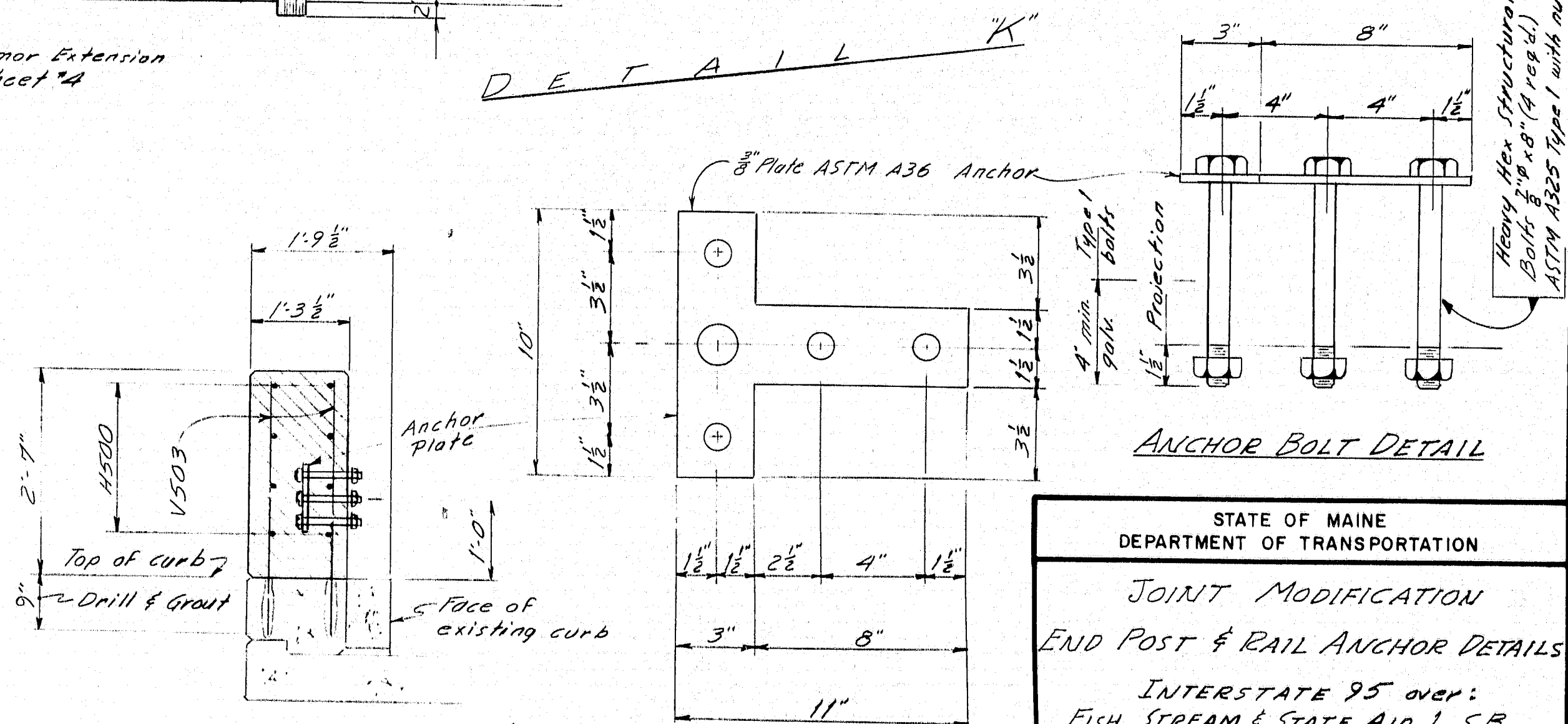
- NOTES**
- | | | | | |
|---|-------|------------|----|----|
| 1 | MAINE | IR-95-9109 | 30 | 34 |
|---|-------|------------|----|----|
1. Reinforcing steel shall have a minimum cover of 2 inches unless otherwise indicated.
 2. Removing of existing reinforcing steel shall be at a minimum.
 3. Existing reinforcing steel to remain shall be cleaned as directed prior to placing new concrete.
 4. Mortar for grouting dowels shall contain an approved non-shrink additive. Payment for drilling and grouting of dowels shall be considered incidental to Item 503.13, Reinforcing Steel, Placing.
 5. ~~Additional holes required in guard rail post #1's may be made by drilling, punching, or any other method that produces a neat, clean hole of the required size. Burning of holes will not be allowed.~~
 6. After installation of guard rail is complete, upset the thread on the anchor bolts in three places around each bolt at the junction of the nut and the exposed thread, with a center punch or similar tool.

7. Work this sheet with:
BD 201-89 Concrete End Post
BD 301-89 Compression Seal
BD 302-89 Gland Seal

~~Fish Stream & State Aid 1 SB 23° 0'~~
~~Oakfield-Smyrna Road NB 28° 29' 41"~~
~~Oakfield-Smyrna Road SB 21° 31' 30"~~
~~Finney Road SB 33° 57' 06"~~
~~Fort Branch~~
~~Hallowmoke River NB 16° 54' 20"~~



DETAIL "K"



ANCHOR BOLT DETAIL

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

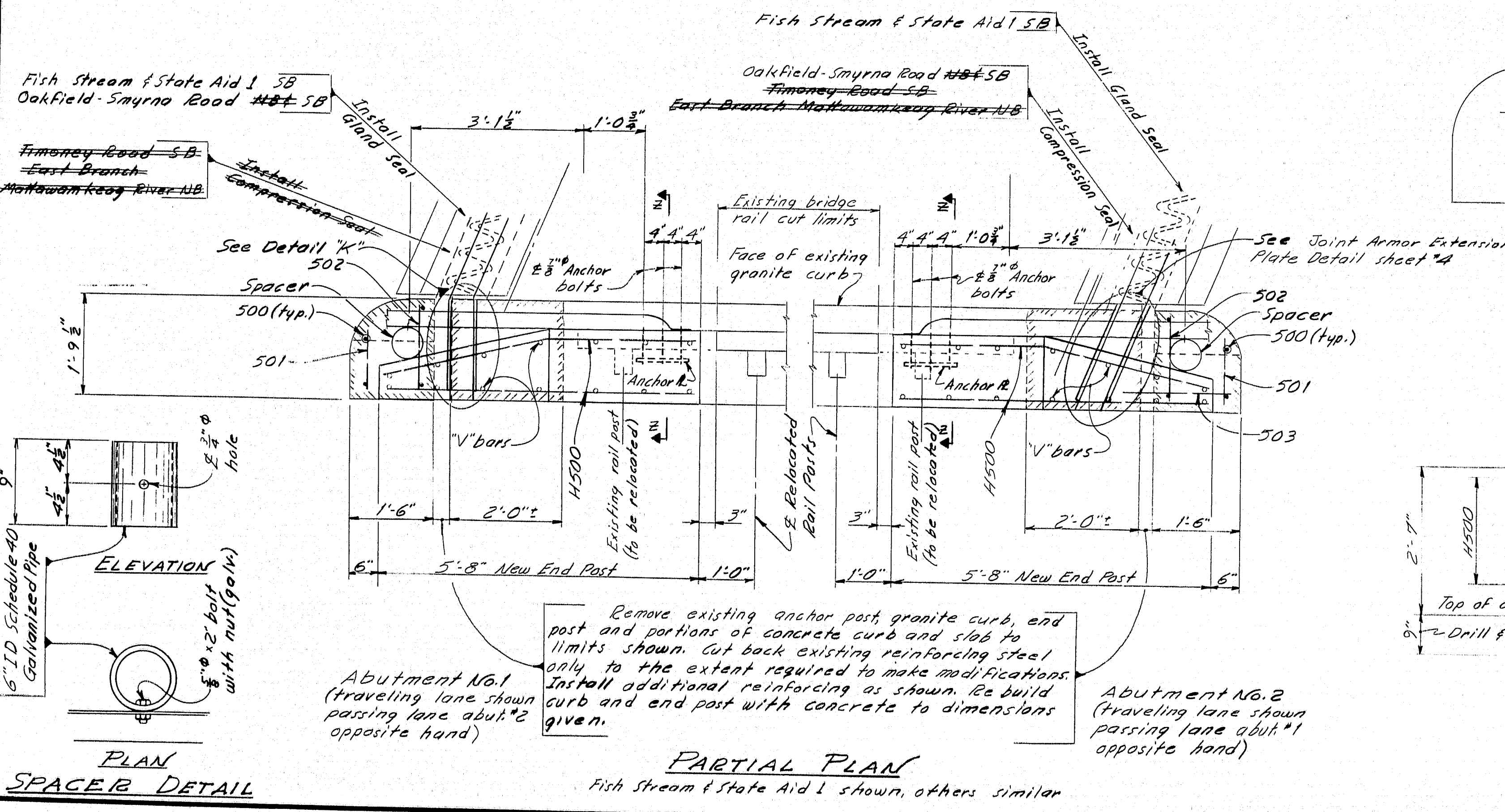
JOINT MODIFICATION

END POST & RAIL ANCHOR DETAILS

INTERSTATE 95 over:
FISH STREAM & STATE AID 1 SB
OAKFIELD-SMYRNA ROAD ~~NB & SB~~
~~TIMONEY ROAD SB~~
EAST BRANCH MATTAHANNAKEG RIVER NB

SHEET 5 OF 8 AUGUSTA, MAINE

1-95-9(109) ISLAND FALLS - OAKFIELD



PARTIAL PLAN

Fish Stream & State Aid 1 shown, others similar

Abutment No. 2
(traveling lane shown
passing lane abut. #1
opposite hand)

Remove existing anchor post, granite curb, end post and portions of concrete curb and slab to limits shown. Cut back existing reinforcing steel only to the extent required to make modifications. Install additional reinforcing as shown. Re build curb and end post with concrete to dimensions given.

Abutment No. 1
(traveling lane sho
passing lane abut.
opposite hand)

PLAN
SPACER DETAIL

PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	RTA	5-08
CHECKED		
REVISED		
FIELD CHANGES		

BRUNING 44-132 45710-1

PLANS
DESIGN - DETAIL
REVISIONS
FIELD CHANGES

REINFORCING STEEL SCHEDULE																											
STRAIGHT BARS														BENT BARS													
MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	O	R	LOCATION	
E. BRANCH MATTAWAMKEAG RIVER SB				FISH STREAM & STATE AID - 1 SB				OAKFIELD - SMYRNA ROAD N/B																			
550	4	1'-5"	Curb	500	16	2'-2"	Dowels	500	16	2'-2"	Dowel																
				501	8	1'-2"	Curb	501	8	1'-2"	Curb																
V500	8	1'-2"	End Post	502	8	1'-5"	Curb	502	8	1'-5"	Curb																
V501	8	1'-5"		503	4	1'-11"	Curb	503	4	1'-11"	Curb																
V502	8	1'-8"	(dowel)																								
V503	32	3'-2"	End Post (dowel)	V500	8	1'-2"	End Post	V500	8	1'-2"	End Post																
				V501	8	1'-5"		V501	8	1'-5"																	
H500	32	5'-4"	End Post	V502	8	1'-8"	(dowel)	V502	8	1'-8"	(dowel)																
				V503	32	3'-2"	End Post (dowel)	V503	32	3'-2"	End Post (dowel)																
				H500	32	5'-4"	End Post	H500	32	5'-4"	End Post																
OAKFIELD - SMYRNA ROAD SB																											
500	16	2'-2"	Dowels																								
501	8	1'-2"	Curb	ROUTE 159 & BARR RD SB				TIMONEY ROAD SB																			
502	8	1'-5"	Curb	550	4	1'-5"	Curb	500	16	2'-2"	Dowel																
503	4	1'-11"	Curb					501	8	1'-2"	Curb																
				V500	8	1'-2"	End Post	502	8	1'-5"	Curb																
V500	8	1'-2"	End Post	V501	8	1'-5"		503	4	1'-11"	Curb																
V501	8	1'-5"		V502	8	1'-8"	(dowel)																				
V502	8	1'-8"	(dowel)	V503	32	3'-2"	End Post (dowel)	V500	8	1'-2"	End Post																
V503	32	3'-2"	End Post (dowel)					V501	8	1'-5"																	
H500	32	5'-4"	End Post	H500	32	5'-4"	End Post	V502	8	1'-8"	(dowel)																
								V503	32	3'-2"	End Post (dowel)																
								H500	32	5'-4"	End Post																
				E. BRANCH MATTAWAMKEAG RIVER NB																							
				500	16	2'-2"	Dowels																				
W. BRANCH MATTAWAMKEAG RIVER SB				501	8	1'-2"	Curb																				
550	4	1'-5"	Curb	502	8	1'-5"	Curb																				
				503	4	1'-11"	Curb																				
V500	8	1'-2"	End Post																								
V501	8	1'-5"		V500	8	1'-2"	End Post																				
V502	8	1'-8"	(dowel)	V501	8	1'-5"																					
V503	32	3'-2"	End Post (dowel)	V502	8	1'-8"	(dowel)																				
				V503	32	3'-2"	End Post (dowel)																				
H500	32	5'-4"	End Post																								
				H500	32	5'-4"	End Post																				

105-51

FWHA REV. NO.	STATE MAINE	PROJECT NUMBER 105-9(109)	SHEET NO. 21	TOTAL SHEETS 34
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TYPE-BENDING DIAGRAMS

All dimensions are out to out of reinf. bar
Bending details and hooks shall conform to the recommendations of the current revision of ACI Standard 318.Δ
Reinforcing Bar: ASTM A615 Grade 60

GENERAL NOTES

- First digit(s) following the letter of the Mark indicates size of reinf. bar.
Mark (A502) bar size - #5
Mark (P1001) bar size - #10
Mark (S603) bar size - #6
- Each truss bar, Type B, may be replaced by two (2) straight bars (one top & one bottom) of the same bar size as the truss bar. Payment in either case shall be based on truss bars as scheduled on plans.

Δ New Bent Bar Type SJ
Δ Revised ACI Standard

REVISIONS	DATE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

REINFORCING STEEL SCHEDULE
INTERSTATE 95 OVER
E. Branch Mattawamkeag River SB
Oakfield - Smyrna Road SB
W. Branch Mattawamkeag River SB
Fish Stream & State Aid 1 SB
Route 159 & BARR SB
E. Branch Mattawamkeag River NB
Oakfield - Smyrna Road NB
Timoney Road SB

SHEET 6 OF 8 AUGUSTA, MAINE